



**Standard Conditions for the Owner/Contractor**

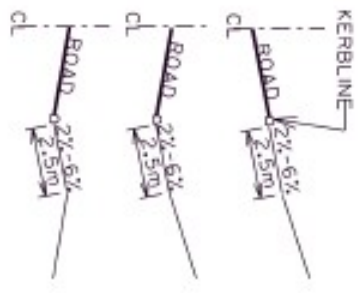
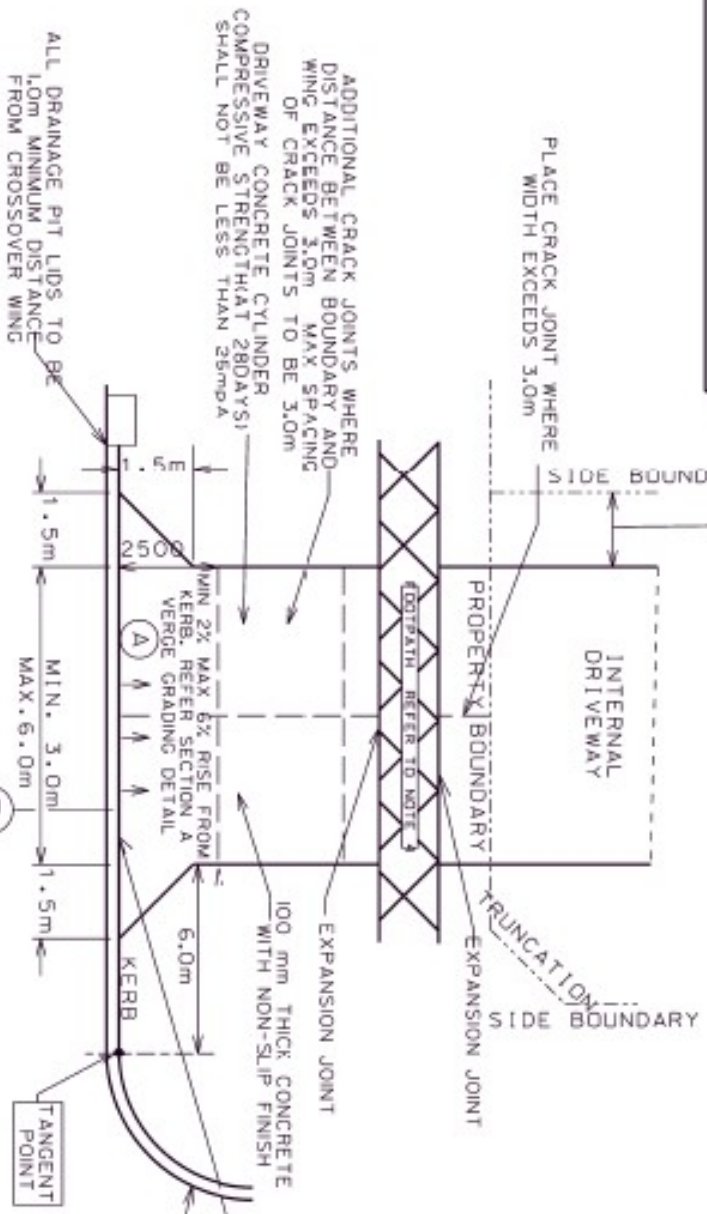
1. Any works within a City of Bunbury thoroughfare (road reserve, PAW, ROW, park, etc.) requires either a Verge Crossover, Verge Treatment or Works in Thoroughfare permit from the Manager Engineering prior to the works commencing.
2. It is the property owner's responsibility to construct the crossover to meet Council's requirements. Ensure the attached standard drawings are provided to the contractor before construction commences. **Crossovers not constructed to the standards may need to be reconstructed and no refund will be issued.**
3. The applicant is responsible for ensuring the public's safety during the construction of the crossover.
4. It is the applicant's responsibility to have dated photographic evidence of the pre-existing condition of the verge prior to the works commencing. Any damaged caused by the works may result in the applicant being fined under the City of Bunbury Local Government and Public Property Local Law.
5. Commercial and Industrial crossovers are to be designed and constructed based on the principles outlined in the specifications and to suit the loads of the vehicles accessing the property.
6. **Mountable kerb must** be installed across crossover entrance for all **residential properties** as per attached drawings. If there is an existing semi or barrier kerb it is to be removed and replace with mountable kerb. Concrete or asphalt fill instead of mountable kerbing in the gutter is not acceptable.
7. The road surface in front of the crossover must be cut using a pavement saw to ensure clean edge of seal at the join with new mountable kerb.
8. There must be a smooth transition from the existing kerb to the new mountable kerb.
9. Crossover to be constructed at a + 2% to +6% grade from the kerb for a minimum distance of 2.5 metres from the back of the mountable kerb. (refer to note 4 above)
10. Verge levels **must not** be altered without prior written approval from the Manager Engineering.
11. Where residential crossovers are to be constructed across existing footpaths the following requirements are to be met:
  - i. Existing path is to remain undisturbed and the driveway constructed to match the existing levels of the path.
  - ii. Where the existing slab footpath requires upgrading check with Engineering Services prior to removing path. Reinstate all gaps with concrete, minimum 50mm thick, to provide a smooth transition from the slab footpath to the driveway. Maximum path grade is 1:14.
  - iii. Steps and lips in paths are not acceptable.For commercial or industrial properties the crossover will be required to be strengthened to cater for the type of vehicles accessing the property.
12. Residential crossovers to be constructed to a minimum width of 3m for a single crossover and 6m maximum for double crossovers, excluding wings. Commercial or Industrial properties crossover widths **may** have a maximum width of up to 10m, subject to it being to the satisfaction of the City of Bunbury. **Applications must be made in writing to the Manager Engineering for any alteration to this standard.**
13. Multiple crossovers for residential properties will only be approved where the R-coding is less than R30, and it does not cover greater than 40% of the verge frontage of the property. The total aggregate of multiple crossovers is to be no greater than 9m.
14. All proposed internal grades of the driveway need to comply with the Australian Standard AS/NZS2890 and is not to exceed 25% without prior written approval from the Manager Engineering.

Standard Conditions Continued on page 3

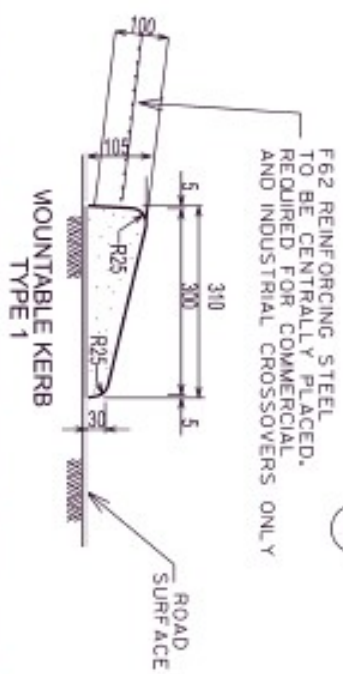
15. A crossover **shall not** be constructed closer than 6m from the tangent point of a corner lot (as indicated on the following crossover drawings). This measurement is made to the straight of the crossover, not at the wing. The City notes that cottage lots may not be able to achieve this requirement, therefore outlines for these lots are that they need to achieve a separation as close to 6m as practical.
16. All drainage pit lids are to be located a minimum 1m distance from crossover wings and will not be permitted within the crossover. Contact the City of Bunbury's Works & Services Department to resolve any issues with this requirement.
17. Crossovers abutting Main Roads require the approval of Main Roads Western Australia (Ph 9724 5600).
18. Relocation or alteration to services (eg: water, telephone, gas, drainage, signage, etc.) will be at the owner's cost and undertaken by the relevant Authority subject to their approval.
19. Transitions from a crossover to existing footpaths shall be to a maximum grade of 1:14 and ensure that no trip hazards are present.
20. Edges of the crossover are to be backfilled, level with the existing ground and the verge left in a tidy condition prior to final inspection.
21. All disused and redundant vehicle crossovers must be removed and the area reinstated to the satisfaction of the City of Bunbury.
22. Maintenance of the crossover is the property owner's responsibility.
23. Payment of a refund will only be made for the **first** crossover to be constructed to the lot. Refunds will not be paid for additional, replacement or crossovers which do not comply with the City's standards.

TO BE READ IN CONJUNCTION WITH STANDARD CONDITIONS FOR ALL CROSSOVER TYPES

PREFERRED MINIMUM CLEARANCE FROM SIDE BOUNDARIES: 1.5m  
 ABSOLUTE MINIMUM CLEARANCE FROM SIDE BOUNDARIES: 0.5m  
 (AT COUNCIL'S DISCRETION)  
 EXCEPT FOR CORNERS WHERE THE MINIMUM DISTANCE FROM THE TANGENT POINT (AS HIGHLIGHTED) IS 6.0m, FOR COTTAGE LOTS PLEASE REFER TO NOTE 6.



• FOR COTTAGE BLOCKS, PLEASE REFER TO NOTE 6.



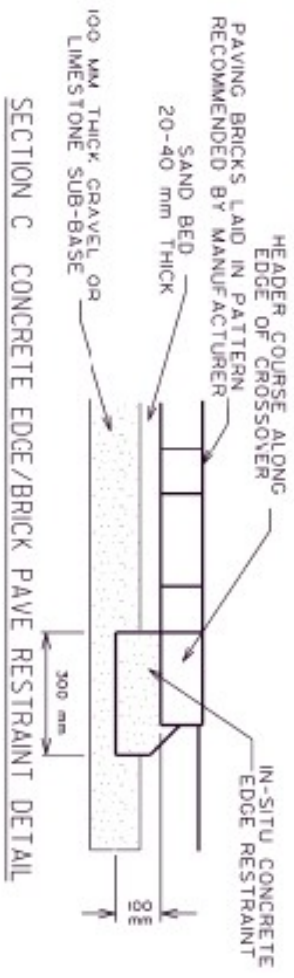
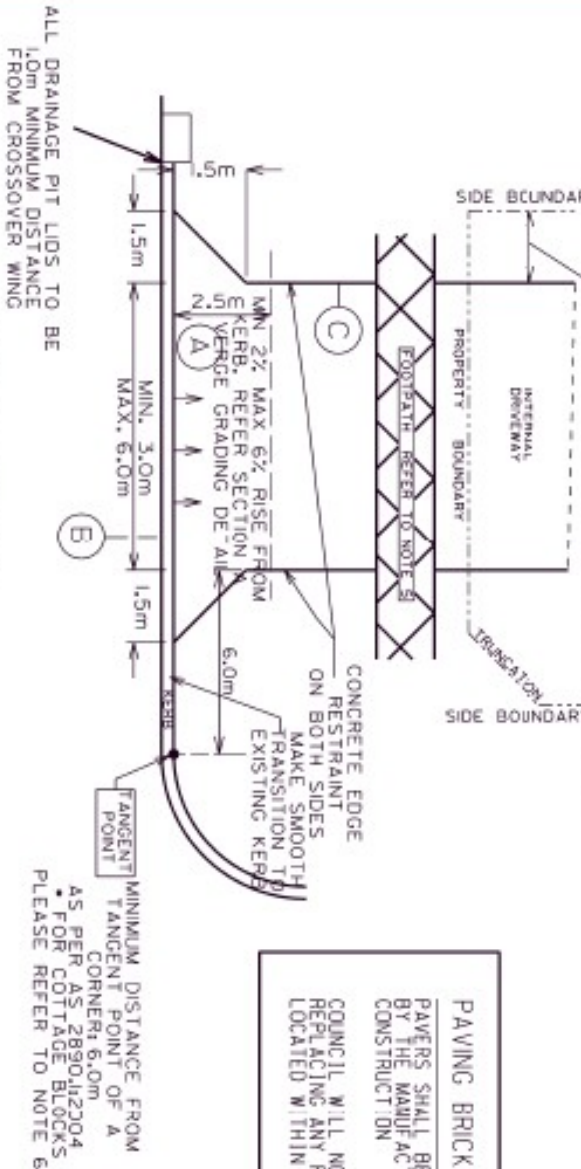
SECTION B MOUNTABLE KERB/CONCRETE DETAIL

<b>CITY OF BUNBURY</b>			
<b>CONCRETE CROSSOVER DETAILS</b>			
SCALE: N.T.S.	DESIGN: W.P.	DRAWN: D.L.N.	CHECKED: W.P.
APPROVED: BEATRICE PLANT 06/2008	REVISION: G. J. NOLAN 06/2008	DATE: 08/2001	DATE: 08/2001
CITY ENGINEER		MISC - 01 - 03	
DATE		SHEET 1 OF 1	

TO BE READ IN CONJUNCTION WITH STANDARD CONDITIONS FOR ALL CROSSOVER TYPES

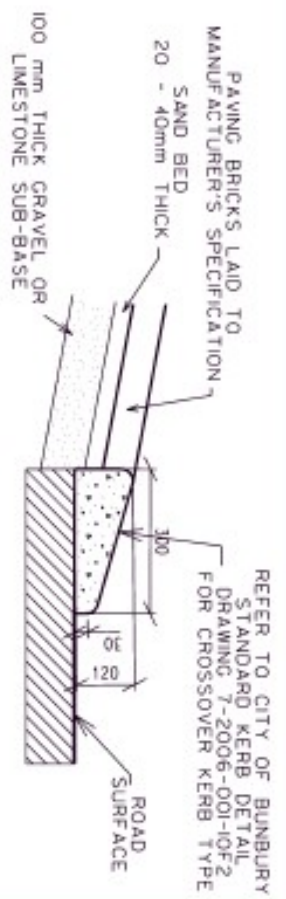
PREFERRED MINIMUM CLEARANCE FROM SIDE BOUNDARIES: 1.5m  
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EXCEPT FOR CORNERS WHERE THE MINIMUM DISTANCE FROM THE TANGENT POINT (AS HIGHLIGHTED) IS 6.0m, FOR COTTAGE LOTS PLEASE REFER TO NOTE 6.

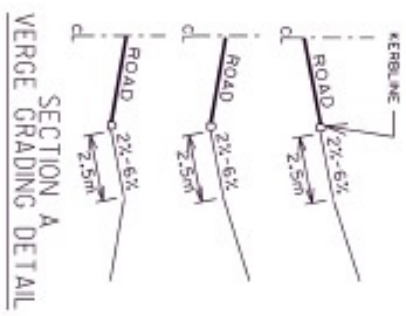


SECTION C CONCRETE EDGE/BRICK PAVE RESTRAINT DETAIL

**PAVING BRICKS**  
 PAVERS SHALL BE OF A TYPE RECOMMENDED BY THE MANUFACTURER FOR USE IN DRIVEWAY CONSTRUCTION  
 COUNCIL WILL NOT ACCEPT LIABILITY FOR REPLACING ANY PRIVATE BRICK PAVING LOCATED WITHIN THE ROAD RESERVE



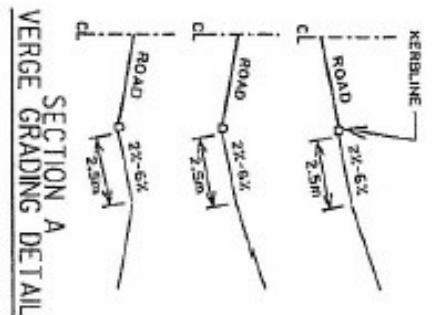
SECTION B MOUNTABLE KERB/BRICK PAVE DETAIL



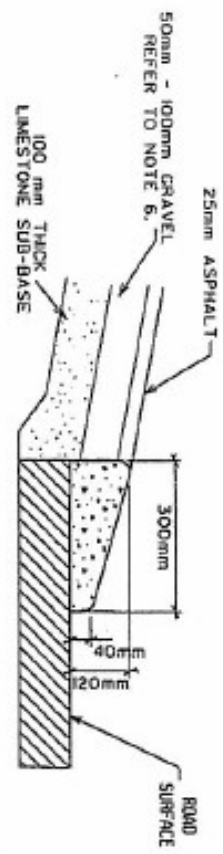
SECTION A VERGE GRADING DETAIL

<b>CITY OF BUNBURY</b>			
<b>RESIDENTIAL</b>			
<b>BRICK-PAVED CROSSOVER DETAILS</b>			
SCALE N.T.S.	DESIGN W.P.	DRAWN D.L.M. 06/2008	REVISION G.J. 06/2008
APPROVED CITY ENGINEER	DATE	PROJECT BEATRICE PLANT 06/2008	DATE 02/03/22
		G.MILAN KERB PROFILE 28/02/11	
		A. 02/03/22	
		MISC - 01-04	
		SHEET 1 OF 1	

TO BE READ IN CONJUNCTION WITH STANDARD CONDITIONS FOR ALL CROSSOVER TYPES



SECTION B MOUNTABLE KERB/ASPHALT DETAIL



ASPHALT CROSSOVERS

KERBING

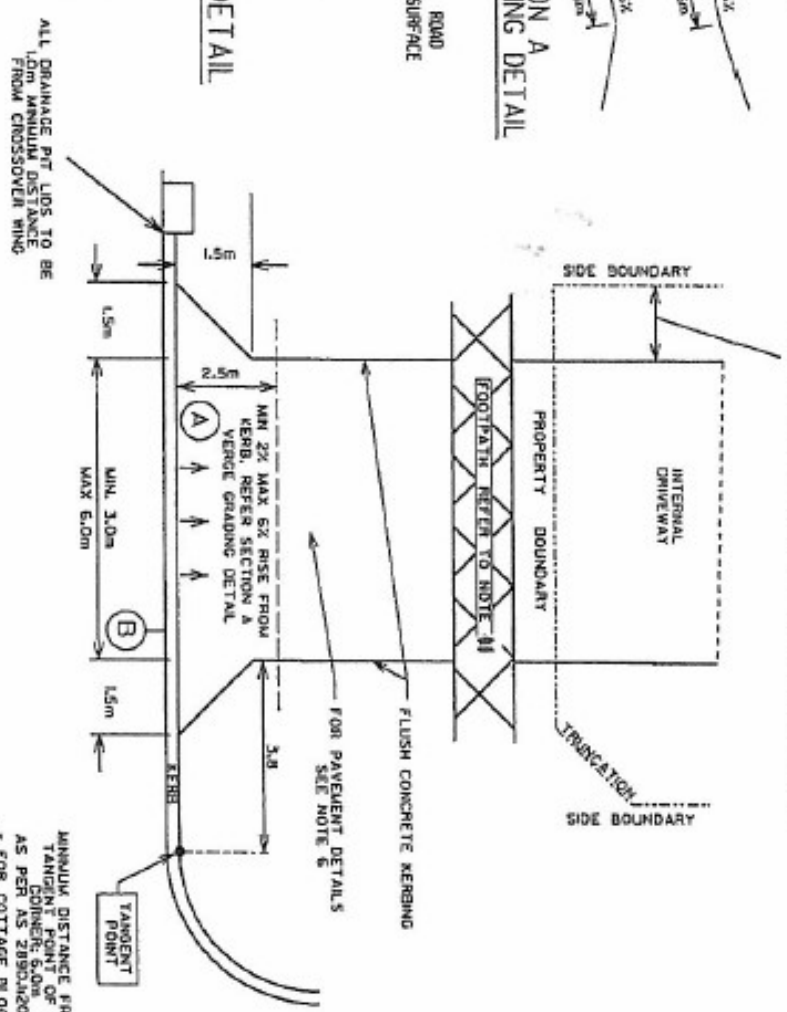
REMOVE EXISTING SEMI OR BARRIER KERBING AND REPLACE WITH MOUNTABLE KERBING. MAKE SANDOTH CONNECTION TO EXISTING KERBING. CONCRETE OR ASPHALT FILL INSTEAD OF MOUNTABLE KERBING IN THE CUTTER IS NOT ACCEPTABLE

CONSTRUCTION

THE CROSSOVER IS TO BE CONSTRUCTED AS FOLLOWS:

- A. 100mm COMPACTED LIMESTONE SUB BASE
- B. 50mm COMPACTED GRAVEL OR ROAD BASE
- C. 25mm ASPHALT SEAL

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 EXCEPT FOR CORNERS WHERE THE MINIMUM DISTANCE FROM THE TANGENT POINT (AS HIGHLIGHTED) IS 6.0m, FOR COTTAGE LOTS PLEASE REFER TO NOTE 6.



MINIMUM DISTANCE FROM TANGENT POINT OF A CORNER: 6.0m AS PER AS 2890.1:2004 - FOR COTTAGE BLOCKS, PLEASE REFER TO NOTE 6.

<p>CITY OF BUNBURY</p>		<p>RESIDENTIAL</p> <p>ASPHALT CROSSOVER DETAILS</p>	
SCALE	N.T.S.	DESIGN	W.P.
DATE	06/2008	REVISION	G.J. MOYLAN 06/2008
<p>CITY ENGINEER</p>		<p>DATE</p>	
<p>BEARANCE PLAN 06/2008</p>		<p>MISC - 01 - 05</p>	
<p>SHEET 1 OF 1</p>		<p>DATE</p>	